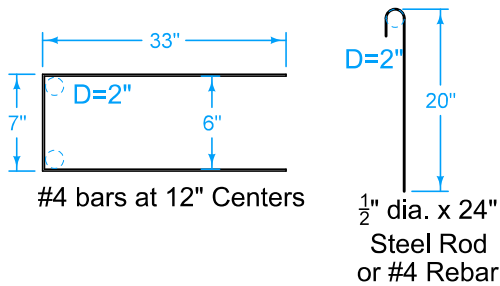
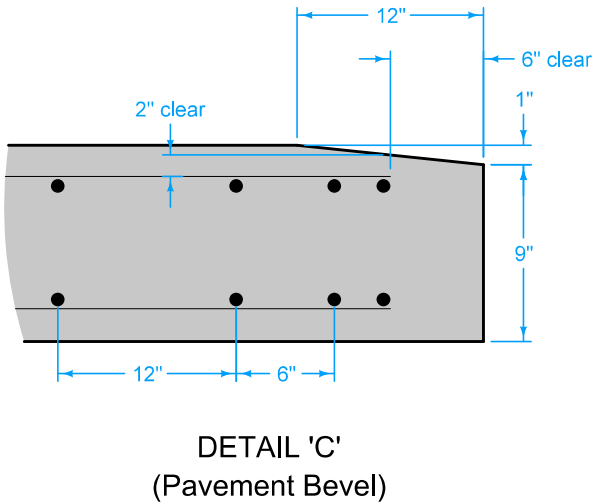
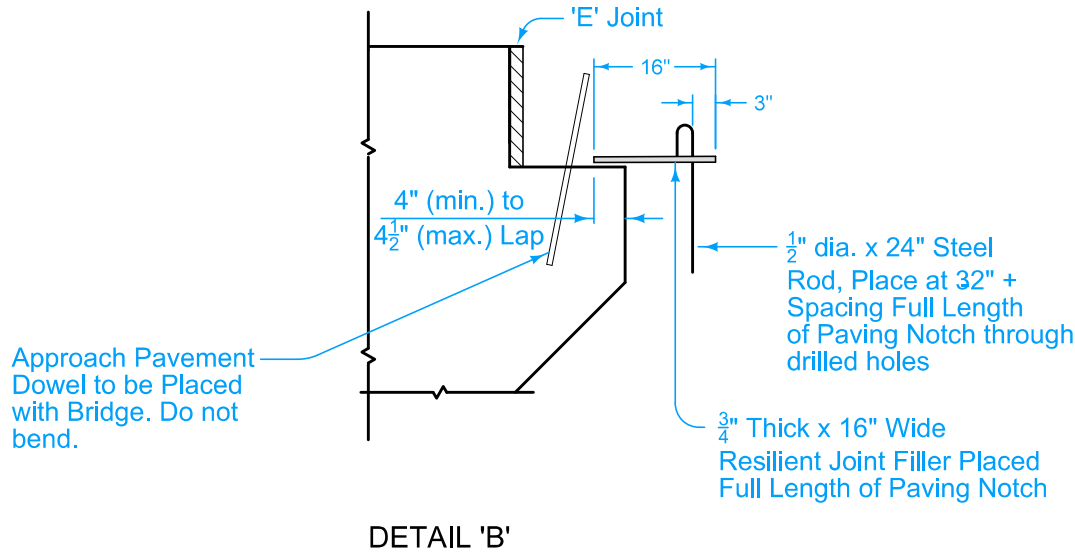
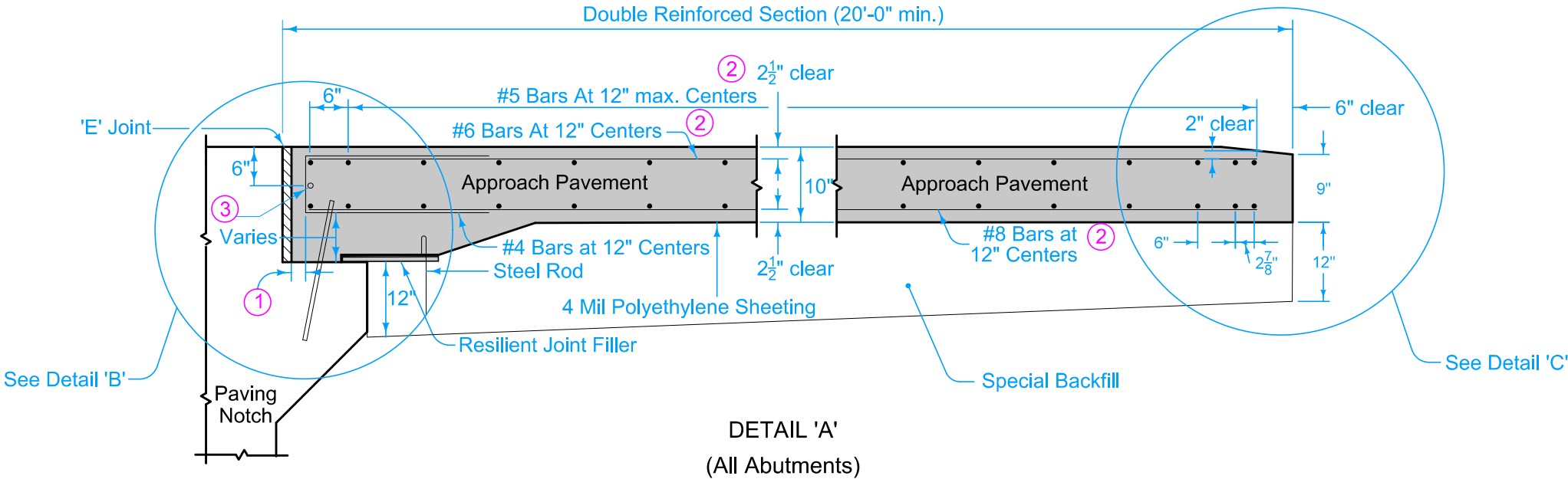


For joint details, refer to PV-101.

For curb details, see Detail 'F'.



All transverse bars are #5.

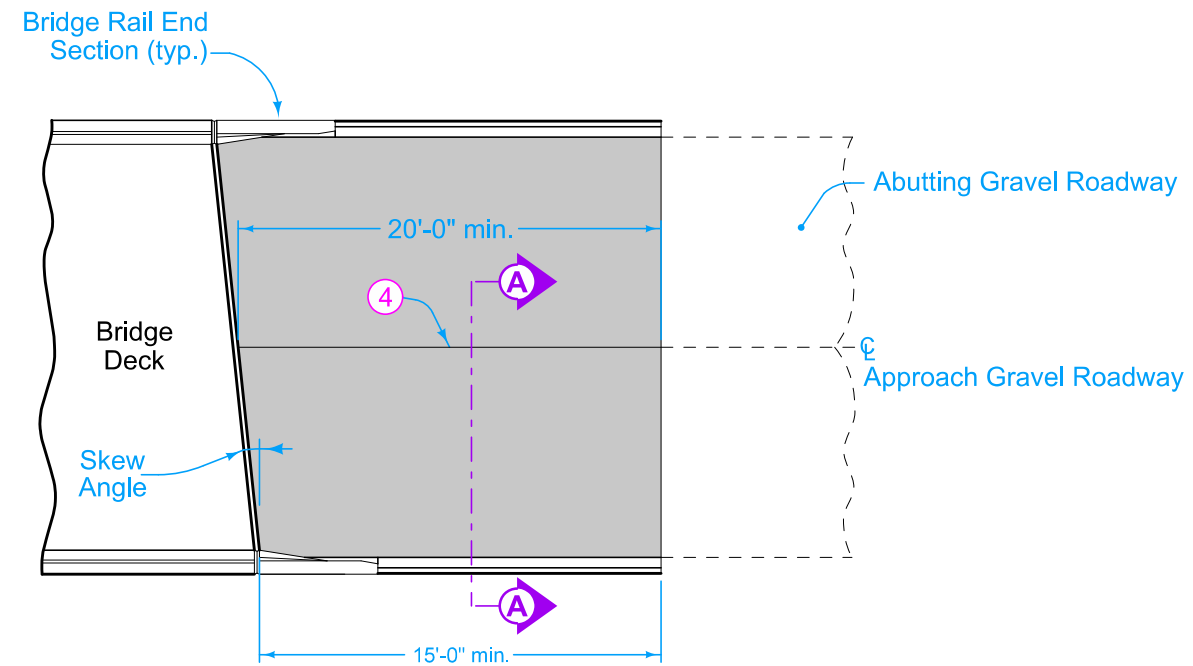
- ① 2" min. to 2 1/2" max. clear to bent bar.
- ② Minimum lap length: #5 Bars - 38"  
#6 Bars - 45"  
#8 Bars - 59"
- ③ If bridge is skewed, place additional #5 bar parallel to skewed face.



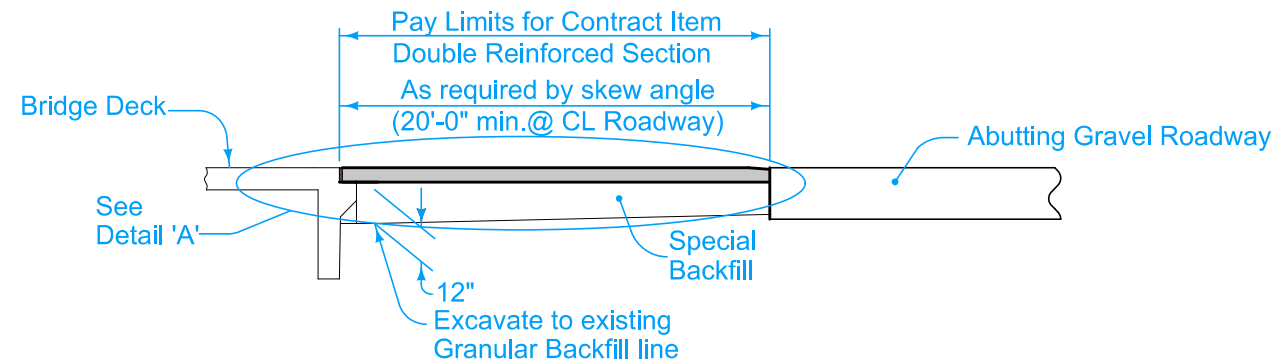
Possible Contract Item:  
Bridge Approach, BR-241  
Longitudinal Grooving in Concrete, Bridge Deck  
Longitudinal Grooving in Concrete, Pavement

Possible Tabulation:  
112-6

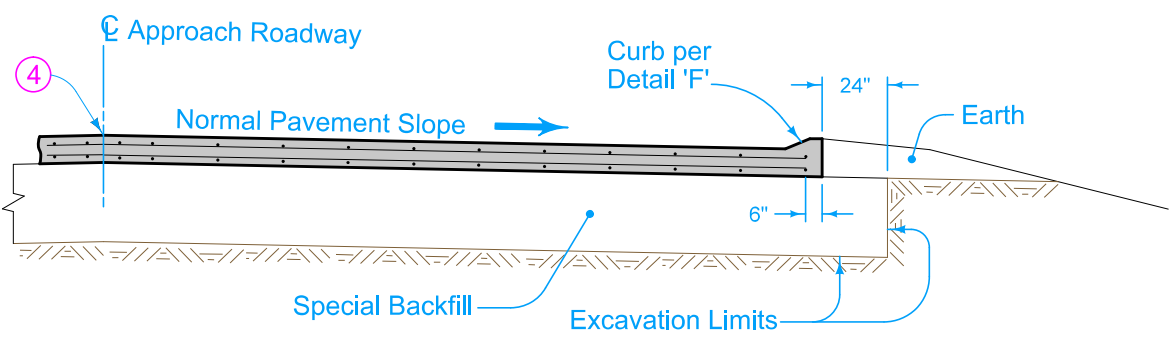
 IOWA   DOT		REVISION	
STANDARD ROAD PLAN		3	10-21-25
		BR-241	
		SHEET 1 of 3	
REVISIONS: Changed 'KS-2' joint to 'BT-2' joint.			
			
APPROVED BY DESIGN METHODS ENGINEER			
DOUBLE REINFORCED 10" APPROACH ON GRAVEL ROADS			



PLAN VIEW



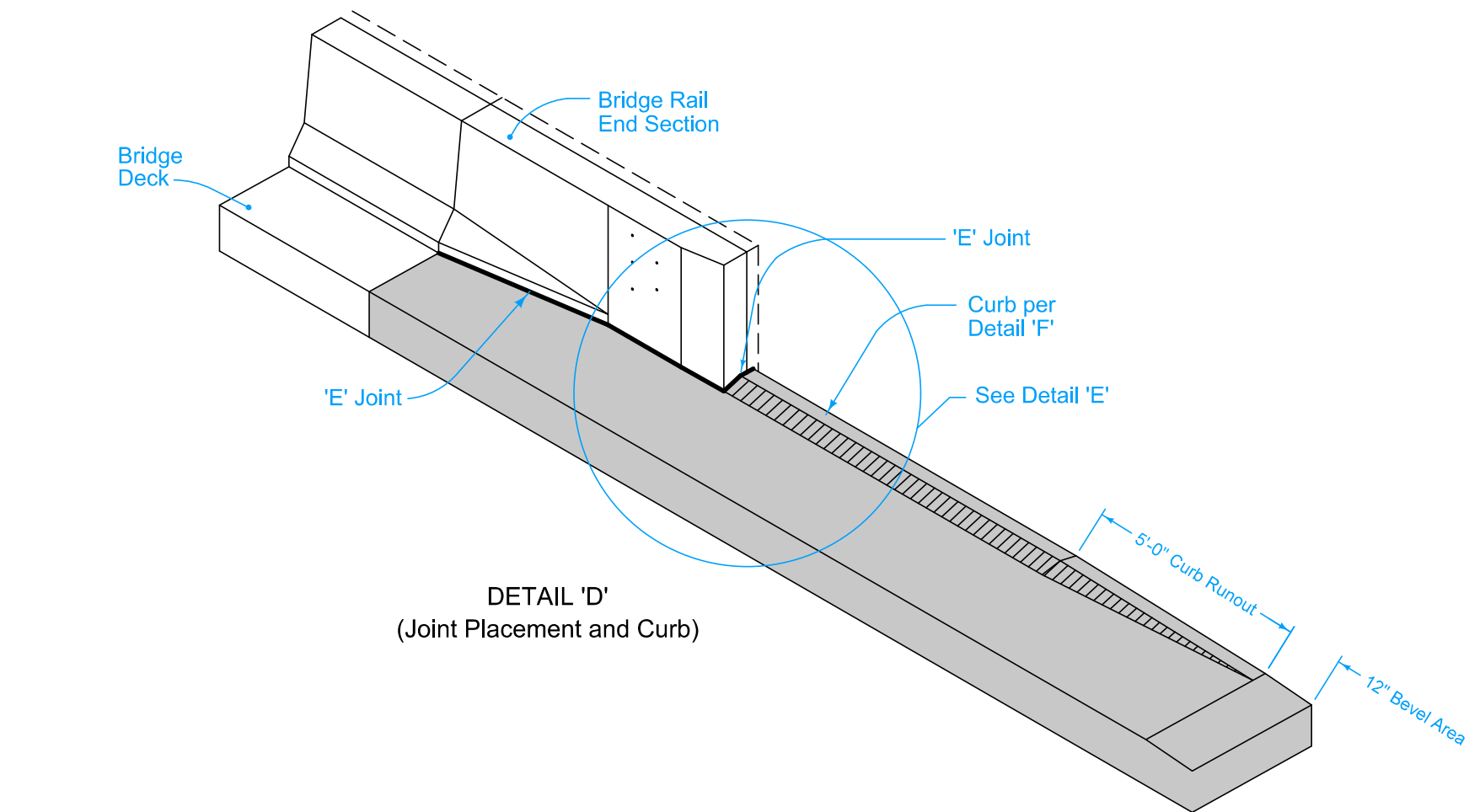
SECTION THRU CENTERLINE



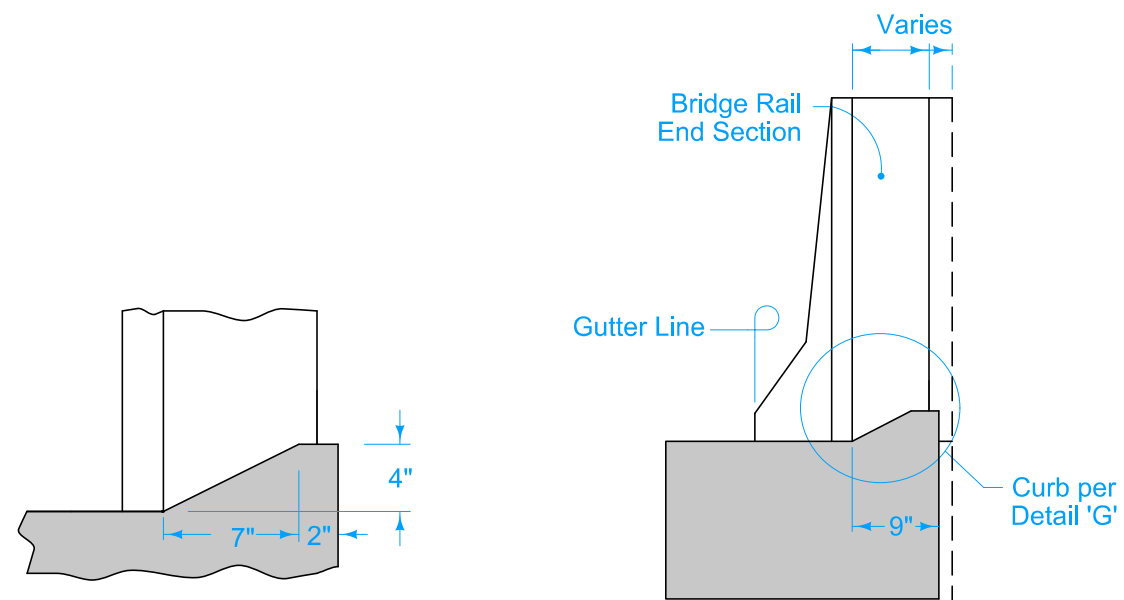
SECTION A-A

4 Longitudinal Joint (PV-101):  
Single pour - Saw cut joint per Detail B.  
Two pours - Use 'BT-2' joint.

<b>IOWA DOT</b> <b>STANDARD ROAD PLAN</b>	REVISION	
	3	10-21-25
	<b>BR-241</b> SHEET 2 of 3	
REVISIONS: Changed 'KS-2' joint to 'BT-2' joint.		
APPROVED BY DESIGN METHODS ENGINEER <i>Shawn Miller</i>		
<b>DOUBLE REINFORCED 10" APPROACH ON GRAVEL ROADS</b>		





DETAIL 'D'  
(Joint Placement and Curb)



DETAIL 'G'

DETAIL 'E'  
(Back of Curb Placement)

 IOWA   DOT	REVISION	
	3	10-21-25
	BR-241	
STANDARD ROAD PLAN	SHEET 3 of 3	
REVISIONS: Changed 'KS-2' joint to 'BT-2' joint.		
		
APPROVED BY DESIGN METHODS ENGINEER		
DOUBLE REINFORCED 10" APPROACH ON GRAVEL ROADS		