

Example 4 – Intersection at Divided Highway (Raised Islands)

This example demonstrates the jointing design process for a four-way intersection with raised islands at a divided highway. The process is basically the same as a T-intersection with raised islands, with the addition of a paved median opening to joint.

As with the T-intersection, start out by placing the longitudinal joints that are predetermined by the lane pavement width. After doing this, place longitudinal joints through the opening (see Figure 1). The edges of the left-turn lanes define the location of two of these joints. The remaining longitudinal joints in the opening are spaced roughly a lane width apart—somewhere in the range of 10 to 16 feet is acceptable.

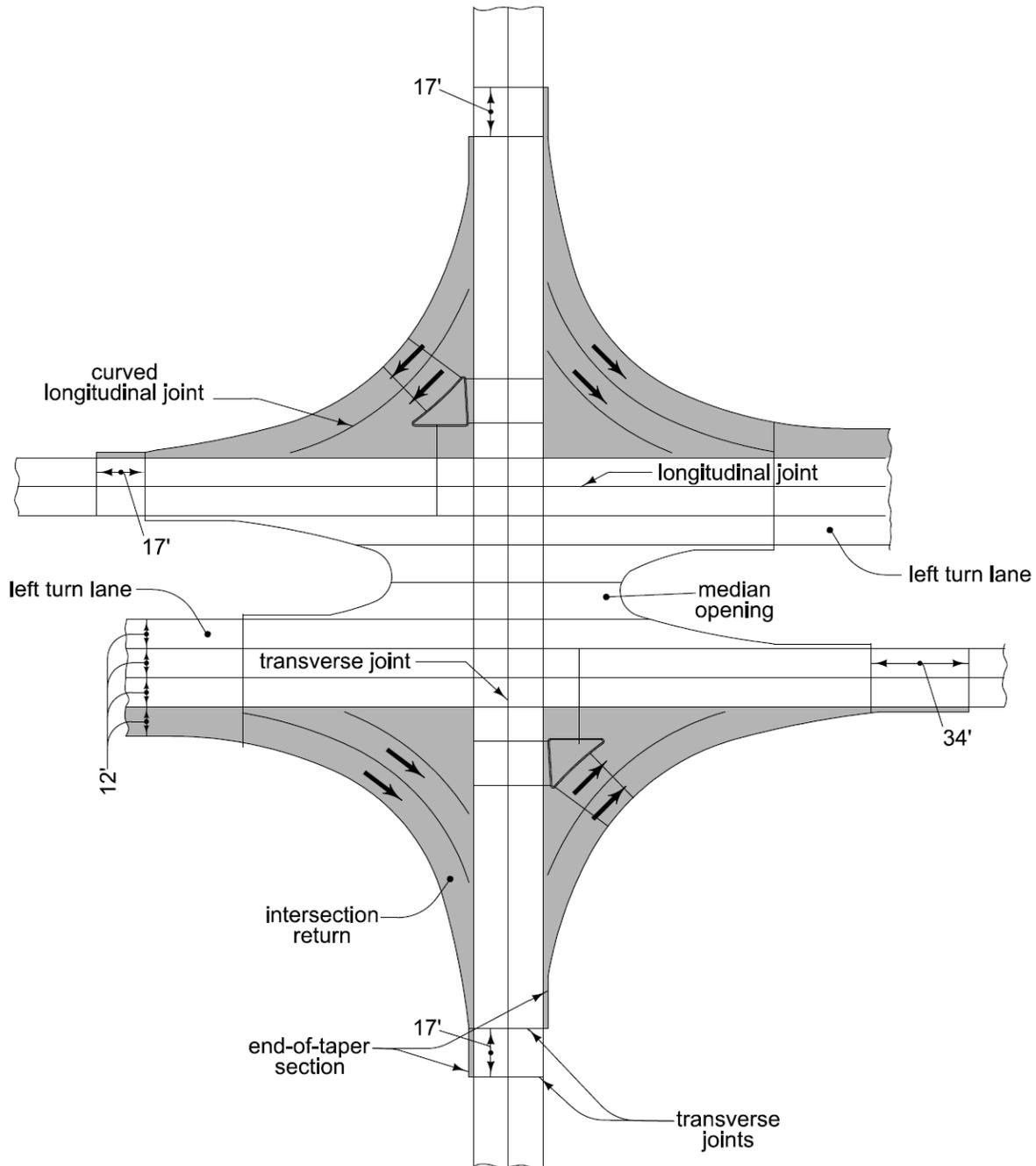
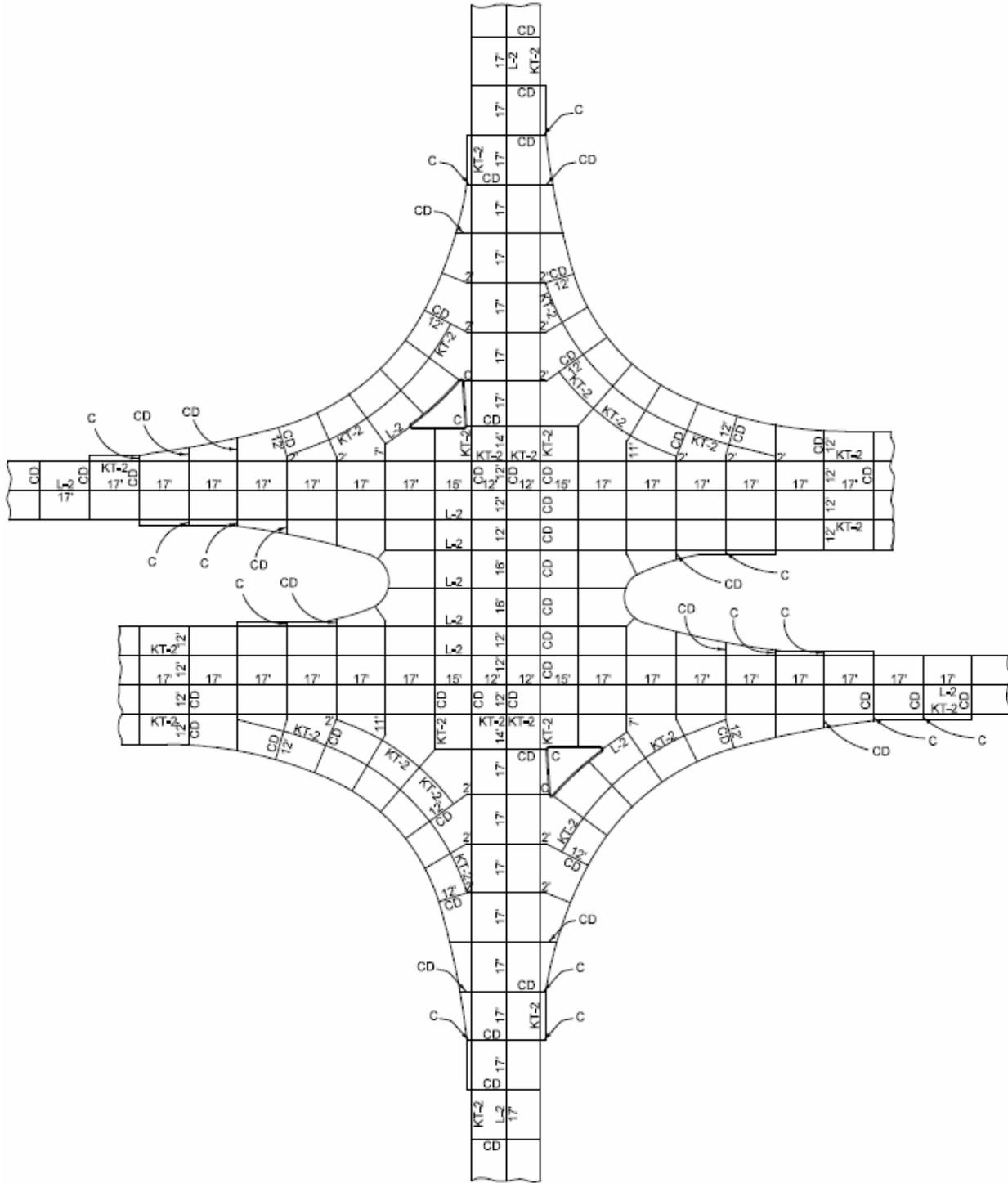


Figure 1: Placement of predetermined and difficult joints.

After this, the process is basically the same as the T-intersection:

- Place the transverse joints at the end-of-taper sections.
- Place the curved longitudinal joints in the return.
- Place the transverse joints around the islands. Figure 1 illustrates the design through this point.
- Place the remaining transverse joints and extend them into the returns and into the median opening. Refer back to the T-intersection example for details on how the joints should intersect with the free edge of the pavement and with other joints.
- Label the joints.

Figure 2 illustrates the final jointing layout.



Note: All longitudinal joints will be either KT-2 or L-2 unless indicated otherwise.

Figure 2: Final jointing layout.